

## Article 7. Access & Parking

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### 7.01 Intent & Applicability

- A. **Intent.** The intent of the Access and Parking standards is to:
1. Emphasize the importance of site access for multiple modes of transportation.
  2. Preserve streetscape design and street functions by coordinating access along block faces and internal to blocks.
  3. Provide the optimal amount of vehicle parking for individual sites, recognizing that too much and too little parking each have negative impacts.
  4. Create access and parking standards appropriate to the context of the site, considering surrounding development patterns and street design.
  5. Ensure appropriate site design features that mitigate the physical and aesthetic impact of parking on streetscapes and surrounding sites.
  6. Maximize opportunities for on-street parking, shared parking or reduced parking rates where appropriate, and reduce the inefficiency from underutilized and redundant surface parking on adjacent sites.
  7. Promote parking designs that minimize runoff, decrease heat island effects, incorporate low impact design features, and infiltrate storm water into the ground.
- B. **Applicability.** Access and parking shall be shown on site plans, according to the application requirements in Article 2. Specifically the standards in this article apply to:
1. All new development, buildings or uses on a site.
  2. A change of use, or additions to existing buildings, that would require 25% or more additional required parking than the previous condition.
  3. Where additional parking is required for an existing lot or building, the parking design standards shall only apply to the newly constructed parking; except, when more than 50% of a parking area is reconstructed, all parking and access shall comply with this section.
  4. The access standards shall not limit the location of any existing access, except:
    - a. In conjunction with a public streetscape project;
    - b. Where an entire site is redeveloped or when more than 50% of a parking area is added, reconstructed, or similarly impacted by development; or
    - c. Where the existing access is determined to be a danger to public safety in association with a development application or street project.

**7.02 Access & Circulation**

A. **Driveways.** Driveways shall be designed according to the following standards.

1. **Driveway Design Objectives.** Driveway location and spacing shall be limited based on the following standards and guidelines:
  - a. Wherever feasible, adjacent lots with a similar land use shall use shared access to preserve the streetscape and eliminate conflicts with pedestrians and vehicles.
  - b. Direct access to major arterial street shall be permitted only when there is no other reasonable access to the street system, after considering alternatives such as access from side streets, shared driveways, common frontage lanes, rear alleys, or internal access streets.
  - c. The frontage design standards on a particular lot or block in Sections 5.03 and 6.03 Frontage Design may put further restrictions on the width, location, or extent of driveways in the frontage area.
  - d. Where driveway spacing standards for streets limit or prohibit access, shared driveways, common access lanes or alleys internal to blocks shall be used.
  
2. **Driveway Location Standards.** Except for where shared or common access is permitted and executed through easements, driveways shall be set back from side or rear lot lines as stated in Table 7-2: Driveway Setbacks.

<b>Table 7-2 Driveway Setbacks</b>			
<b>Access</b>	<b>Setback from side or rear lot line</b>	<b>Setback from street side corner</b>	<b>Separation from other access</b>
<i>Residential access &lt; 6 units</i>	2' +	40'	35'[1]
<i>Residential access 7 – 40 units</i>	5' +	60'	100'
<i>Nonresidential access and residential access to 41+ units</i>	10+	100'	150'
<i>Shared access</i>	May be on the property line, subject to easements.		
<i>Alley access</i>	Front-loaded driveways may be prohibited on blocks with alley access; there are no access width or separation requirements for access from an alley.		

[1] Driveway spacing may be averaged along a block for residential lots or on local streets to allow the best arrangement considering grades, streetscapes, and building and lot layouts. This may result in two adjacent lots having access near the same lot line on one side (2' side setback on each), while greater separation between driveways is provided between lots on the opposite sides.

3. **Through Access Drives.** Any single project, lot, or site greater than 5 acres, or lots where access is constrained by driveway standards, shall provide a system of through access drives that establish access and circulation within the site. (See Figure 3-2 in Section 3.01). Through access drives:
  - a. Shall be laid out to organize the site into smaller internal blocks between 1 and 4 acres.
  - b. Shall be designed to mimic public street cross sections in Section 3.01, including sidewalks, landscape amenities, on-street parking and travel lanes.
  - c. May be treated as public streets for determining the proper location, orientation and design of sites, buildings, and utilities within the project.
  - d. Trail, greenway, or pedestrian passages meeting the standards of Section 3.02 may account for a portion of this internal circulation network, provided it connects

buildings, open spaces, and internal streets with similar networks external to the site and presents a logical connection point for pedestrians and bicycles.

4. **General Design Standards.** All access shall meet the following design standards.
  - a. Sufficient on-site storage to accommodate queued vehicles waiting to park or exit without interfering with street traffic.
  - b. Provisions for circulation between adjacent parcels shall be provided by through access drives, cross access easements, and other shared access provisions to protect the function, design, and character of public streets.
  - c. Driveway spacing and design shall be located so that safe ingress and egress is provided, considering the function and design speed of the street from which the access is provided, and minimizing potential conflicts of all modes of transportation, including pedestrians, bicycles, and vehicles.
  - d. Landscape, buildings, and other site elements at access points shall be designed to meet the sight distance requirements of Section 3.01.D.2, Sight Distances.
  - e. Any access from a state highway shall only be permitted as authorized and approved by the Colorado Department of Transportation.

**B. Sidewalks.**

1. **Sidewalk Design Objectives.** Development sites shall include direct sidewalk connections and circulation at the same or greater frequency as provided for vehicles. Sidewalks shall connect public entrances of buildings and sites to the following, in the most direct manner possible:
  - a. Sidewalks in the public streetscape or along through access drives.
  - b. Parking areas and any perimeter sidewalks, internal walkways or crosswalks associated with the parking areas.
  - c. Civic or open space, or other common areas designed for active use.
  - d. Transit stops, ride-share, or park and ride location – existing or anticipated.
  - e. Where connections from the main entry to sidewalks in the public streetscapes or through access drives is not practical or would exceed 300 feet, sites shall provide pedestrian connections to the edge of adjacent property where any of the above amenities are closer and more directly accessible.
2. **Sidewalk Width.** Internal sidewalks shall meet the requirements of Table 7-3: Internal Sidewalk Widths.

<b>Table 7-3: Internal Sidewalk Widths</b>	
<b>Location</b>	<b>Minimum Width</b>
<ul style="list-style-type: none"> <li>▪ Generally; OR</li> <li>▪ Any residential property</li> </ul>	5'
<ul style="list-style-type: none"> <li>▪ Along the facade of a commercial building of 5,000 s.f. or less abutting a parking area; OR</li> <li>▪ Along any through access drive</li> </ul>	6'
<ul style="list-style-type: none"> <li>▪ Along the facade of a commercial building of 5,001 s.f. to 19,999 s.f. abutting a parking area; OR</li> <li>▪ A primary route between the street or parking area and the building entrance.</li> </ul>	8'
<ul style="list-style-type: none"> <li>▪ Along the facade of any commercial building of less than 20,000 square feet with a primary entrance, or similar building with significant public and pedestrian access.</li> <li>▪ Along the facade of a commercial building of 20,000 s.f. or more abutting a parking area or with a primary entrance.</li> <li>▪ Any access designed for both pedestrians and bicycles.</li> </ul>	12'
<ul style="list-style-type: none"> <li>▪ Along any parking area with vehicle overhangs.</li> </ul>	+ 2' to other required width

3. **Pedestrian Amenities.** Sidewalks and internal pedestrian circulation shall be separated from moving vehicles with curbs, landscape buffers, curbside parking, or similar elements of the circulation and open space system; except crosswalks or other similar limited segments, which may be distinguished paint, brick, or colored or scored concrete and similar design features that signify pedestrian priority.
- C. **Alternative Compliance.** Alternative compliance to the access and circulation standards in this Section may be authorized according to the process and criteria in Section 2.07, Alternative Compliance, based on the intent and design objectives of this section, and with any of the following additional applicable criteria:
1. The standards, when applied to a particular project or street, will adversely impact the function of the transportation network in the vicinity of the site.
  2. A specific access management study or plan has altered the application of these standards for the particular street segment, or the guidance for traffic control in the Standards and Specifications Manual lead to a different result.
  3. The project warrants a different access design when considering the functional class of the street, the streetscape design on the particular block, and the existing and anticipated adjacent land uses.
  4. In all cases, the alternatives shall be evaluated balancing the streetscape design objectives of the specific street type, traffic conditions of a particular street segment, and bicycle and pedestrian needs.

**7.03 Required Parking**

- A. **Vehicle Parking Rates.** Table 7-4: Required Parking provides minimum parking requirements and general categories apply to all similar uses not specifically listed. Where a use is not similar to a general use in the table or could meet more than one category, the Director shall determine the appropriate classification based on industry guides and the most similar use in terms of scale, format and operation. The following criteria shall be used in interpreting the table:
1. Employee rates shall consider maximum number of employees likely to be on-site at one time.
  2. Square footage rates shall consider leasable floor area or active area dedicated to the particular use. Where this number is not easily or readily determined, 85% of gross floor area may be used.
  3. A seating or capacity rate shall consider total number of seats based on industry standards for typical layouts of buildings or building codes.
  4. Where uses or sites have components of different uses (i.e. hotel with a restaurant), each component shall be calculated under most applicable rate.

<b>Table 7-4: Required Parking</b>	
<i>Use Category / Specific Use</i>	<i>Minimum Parking Rate</i>
<b>Residential</b>	
<i>Accessory Dwelling</i>	1 / unit
<i>Dwellings (detached, manufactured)</i>	2 / unit
<i>Dwellings (attached, multiple, or mixed)</i>	1 / unit (Studio / 1 bedroom)
	1.5 / unit (2 bedroom)
	1.75 / unit (3 bedroom)
	2 / unit (4 + bedroom)
	Director may administratively approve 0.5 / unit for micro units (> 400 s.f.)
<i>Senior Living (independent)</i>	Same as Dwellings (attached, multiple or mixed)

<b>Table 7-4: Required Parking</b>	
<b>Use Category / Specific Use</b>	<b>Minimum Parking Rate</b>
<i>Senior Living (assisted or nursing)</i>	1 / 4 beds + 1 per employee
<i>Group Home (assisted)</i>	Same as Dwellings (detached, manufactured)
<i>Group Home (protective or rehabilitative)</i>	1 / 1 bed + 1 per employee
<i>Group Home (emergency shelter)</i>	1 / 8 beds + 1 per employee
<b>Public / Civic</b>	
<i>Assembly</i>	1 / 3 seats
<i>Public Safety / Services</i>	1 / 400 s.f.
<i>Library</i>	1 / 600 s.f.
<i>Museum</i>	1 / 1,000 s.f.
<i>School</i>	2 / class (elementary or junior) 1 / 4 students + 1 / employee (senior or higher education) OR 1 / 4 seats of all auditorium or even space, whichever is greater
<b>Commercial</b>	
<i>Retail – Small (under 3K)</i>	1 / 500 s.f.
<i>Retail – General (3K+)</i>	1 / 250 s.f.
<i>Lodging - B&amp;B</i>	1 / guest room + 1 for operator or owner
<i>Lodging - Hotel / Motel</i>	1 / guest room + 1 / 200 s.f. of restaurant + 1 / 8 seats of meeting space
<i>Medical Care</i>	1 / 200 for all general office and service areas + 1 / bed (admittance permitted)
<i>Office</i>	1 / 300 s.f.
<i>Services ( i.e. medical services, day care, bank, vehicle repair, or similar uses with frequent customer service)</i>	1 / 250 s.f.
<i>Restaurant, bar or night club</i>	1 / 100 s.f.
<i>Health and Fitness Center</i>	1 / 150 s.f.
<i>Recreation and Entertainment</i>	1 / 300 s.f. generally 1 / 4 seats of fixed seating areas 1 / active patron station (i.e 4 per lane bowling; 4 per hole golf course; etc.) 1 / 100 s.f. for food and beverage service areas
<b>Industrial</b>	
<i>Manufacturing</i>	1 / 500 s.f. (artisan/limited or light) 1 / 750 (all others) Director may administratively approve 1 / 1000 s.f. or 1 / employee for any large format manufacturing operations where the s.f. of building or site does not reflect the scale of operations or parking needs
<b>Agriculture</b>	
<i>.All uses</i>	Use combination of residential, public/civic commercial and industrial rates based on type and general nature of agriculture activities.

**B. Maximum Parking.** Nonresidential uses shall not provide more than 140% of the minimum required parking without documented evidence of actual parking demand based on studies of similar uses in similar contexts. In addition, any parking permitted over 120% of the minimum shall require mitigation of the potential impacts of additional parking through one or more of the following strategies:

1. Utilize all eligible parking reductions permitted in Section 7.03.C., Parking Reductions.
2. Provide shared parking for other uses on the block or adjacent blocks according to this Section 7.03.D.

3. Use alternative surfaces designed to infiltrate stormwater, and approved by the Public Works Director.
4. Provide additional buffers and site open spaces to screen parking with at least a 10% increase in the open space or buffers required for the parking, and at least a 20% increase in the amount of landscape material required for the parking.
5. Design all parking areas over the 120% minimum as dual-purpose space, such as plazas, playgrounds, or similar event areas for regular and active use of the space during non-peak parking times.
6. Increase the lot open space required for the building and site in Section 5.03 or Section 6.03 by an amount equal to the area of parking over the 120% minimum and locate this open space to limit the impact and visibility of parking.

C. **Parking Reductions.** The parking required by Table 7-4: Required Parking may be reduced depending on context and according to the following strategies:

1. *DT Exempt.* No parking is required in the DT zoning district, The Director may require parking for any nonresidential use over 10,000 square feet provided the location, accessibility, and design of the parking is consistent with the overall planning and urban design objectives of the downtown area, or for any residential uses where the location, density, and residential building types are likely to have an impact on public or business parking in the vicinity.
2. *On-street Parking Credit.* All on-street parking within 300 feet of any lot frontage shall count towards the parking requirement at a rate of 0.5 spaces for every on-street space.
3. *Bicycle Parking Credit.* All bicycle parking designed and located according to Section 7.03.E. may reduce the required vehicle parking at a rate of 1 space for every 2 bicycle parking spaces up to a maximum of 20% of the required vehicle parking or 20 spaces, whichever is less. To be eligible for this credit, the applicant must demonstrate that it is practical to expect significant bicycle access to the site based on: the location and proximity to the broader bicycle transportation network; the design of the site; and the nature of the use and anticipated patrons.
4. *Administrative Adjustment.* The Director may reduce the required parking for any use that requires more than 10 spaces by up to 10% of the required spaces due to the nature of a particular use or any unique circumstances on the site.

D. **Shared Parking.** Required parking may be reduced for any site containing multiple uses, or for adjacent sites with different uses according to Table 7-5, Shared Parking.

1. Any shared parking arrangement shall require an agreement among all landowners participating in the agreement to ensure access, joint use, maintenance, and other operational issues.
2. The agreement shall be recorded for each participating property with the county clerk and recorder. The agreement shall state that it cannot be changed or modified without the approval and signature of the Director.
3. A shared agreement that differs from this table may also be approved based on a joint parking study for the sites and uses demonstrating adequate parking during peak hours for all parties to the agreement.

<b>Table 7-5: Shared Parking</b>					
<b>Use</b>	<b>Percentage of Required Parking by Time Period</b>				
	<b>Weekday</b>		<b>Weekend</b>		<b>All</b>
	<b>6 AM to 5 PM</b>	<b>5 PM to 1 AM</b>	<b>6 AM to 5 PM</b>	<b>5 PM to 1 AM</b>	<b>1 AM to 6AM</b>
<i>Employment</i>	100 %	10 %	5 %	5 %	5 %
<i>Retail or Service</i>	75 %	75 %	100 %	90 %	5 %
<i>Restaurant</i>	50 %	100 %	75 %	100 %	25 %
<i>Entertainment &amp; Recreation</i>	30%	100 %	75 %	100 %	5 %
<i>Place of Worship</i>	5 %	25 %	100 %	50 %	5 %
<i>School</i>	100 %	10 %	10 %	10 %	5 %
<i>Dwellings</i>	25 %	90 %	50 %	90 %	100 %
<i>Lodging</i>	50 %	90 %	75 %	100 %	100 %

E. **Bicycle Parking.** All nonresidential or multifamily uses within 600 feet of an existing or future bicycle route or trail identified in the Comprehensive Plan or other related bicycle transportation plan shall provide bicycle parking spaces according to Table 7-6, Bicycle Parking.

<b>Table 7-6 Bicycle Parking</b>	
<b>Activity</b>	<b>Required Spaces</b>
<i>Primary or secondary school</i>	30% of the student capacity
<i>Retail or office uses</i>	10% of the required vehicle spaces.
<i>Recreation and community facilities</i>	15% of the required vehicle spaces
<i>Other institutional, employment, industrial or entertainment uses</i>	5% of the required vehicle spaces.
<i>Multi-unit Residential Buildings</i>	1 per dwelling unit; 2 per dwelling unit with 3 or more bedrooms

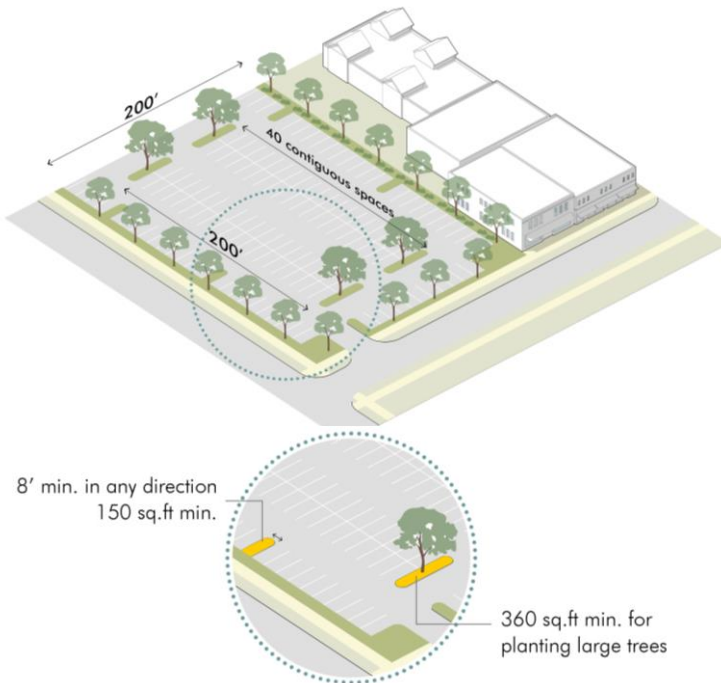
Bicycle parking shall be designed according to the following standards:

1. A structure shall be securely anchored to the ground and usable for both U-locks and cable locks, support a bike at two points of contact to prevent damage to wheels or frames.
2. Bicycle parking for nonresidential uses shall be located within 100 feet of the primary entrance unless the Director approves an alternative location based on the site design and overall vehicle, bicycle, and pedestrian circulating for the site.
3. Bicycle parking facilities may be located in the right-of-way subject to streetscape design plans and the Director approval. Structures shall be designed for some other primary purpose meeting the streetscape standards or be designed with artistic or ornamentation enhancements compatible with the streetscape character at the specific location.
4. On-site structures that serve another primary function, such as light poles or bollards, but are designed to meet these standards, may count to the bicycle parking requirement.
5. Alternative standards and specifications based on recognized industry guidance or best practices for bicycle parking may be approved by the Director through site plan review.

- F. **Accessible Parking.** Accessible vehicle parking spaces shall be provided in accordance with the applicable building codes and the Americans with Disabilities Act (ADA) guidelines for quantity, design, and location.

**7.04 Parking Design**

- A. **Parking Landscape Design Objectives.** Landscape areas required by Table 7-7, Parking Design shall be arranged to achieve the following design objectives:
  1. All parking lot islands landscape buffers shall have the proper allocation of landscape materials required by Article 8 and be arranged to provide shade, infiltrate runoff, soften large expanses of pavement, and screen parking from adjacent streets and property.
  2. Parking rows shall be no more than 15 contiguous spaces without landscape islands (end caps, center islands, or peninsulas), or no more than 3 double-loaded bays without perimeter landscape or a landscape median. This may be adjusted on a specific site plan that results in the same amount of islands and landscape on average for the entire parking lot.
  3. No landscape island shall be less than 8 feet in any dimension and no smaller than 150 square feet.
  4. Any perimeter buffer or center landscape strip that contains a sidewalk shall have at least 5 feet of landscape on each side of the sidewalk, or 8 feet on one side of the sidewalk in order to contribute to the parking landscape requirement.



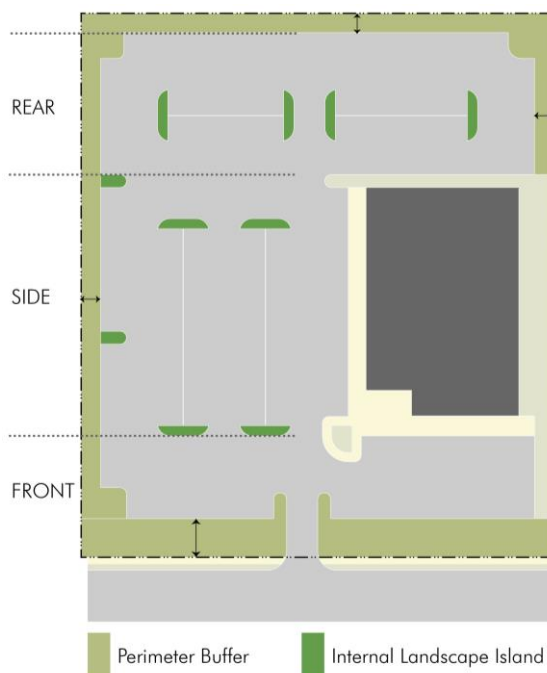
**Figure 7-1 Parking Landscape**  
The required landscape elements should be located to break up the larger expanses of parking and to ensure the survival and maximum mitigating impact of planting in the landscape areas.

- B. **Location, Size and Landscape Area.** On-site parking shall be designed and located in a manner that mitigates negative impacts on streetscapes and adjacent property. The design standards in Table 7-7, Parking Design are based on the number of parking spaces per area and the location on the lot relative to the principal building (front, side, or rear).



Spaces per Parking Block	Front [1]	Side	Rear
151 or more	Must be broken into smaller parking blocks. [2]	10% internal landscape islands; AND 10' perimeter buffer	6% internal landscape island; AND 10' perimeter landscape
51-150	10% internal landscape islands; 10' perimeter buffer; AND 20' front setback buffer	6% internal landscape islands; AND 6' perimeter buffer	6% internal landscape island; AND 6' perimeter buffer
20-50	6% internal landscape islands; AND 6' Perimeter buffer; AND 10' front setback buffer	6' perimeter buffer	6' perimeter buffer, unless abutting an alley
Under 20	6' perimeter buffer; AND 6' front setback buffer	6' perimeter buffer	6' perimeter buffer, unless abutting an alley

- [1] Any surface parking lot in residential districts shall be behind the front building line or setback at least 30' from the front lot line, whichever is less.
- [2] Where individual sites require or provide parking areas larger than the spaces per parking block in this table, parking lots shall be broken into multiple "parking blocks" meeting the size, location, and landscape requirements of this table. These "parking blocks" shall be arranged around perimeter buffers, continuous landscape medians, and through access drives that mimic public streetscapes.



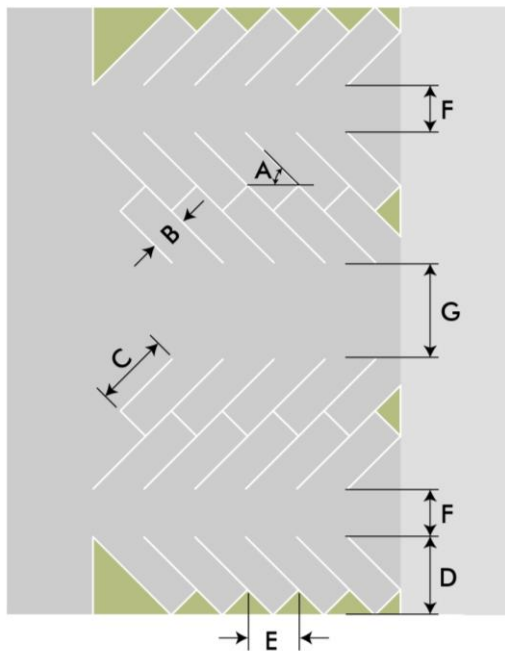
**Figure 7-2 Parking Design**  
Design standards for parking, including buffer and landscape islands, depend on its location in relation to the building and streetscape, and on the size of the parking area. Table 7-7 is based on larger parking areas and parking in the frontage area requiring greater limits or landscape design mitigation than smaller parking areas or parking in the rear of buildings.

- C. **Sidewalks.** In meeting the standards of Sections 7.02.B and 7.04.B, a sidewalk connection shall be provided from the perimeter of the parking lot to the building entrance or building frontage. For parking areas over 200 spaces, a sidewalk connection shall be provided through the parking area and to the building frontage at least every 400 linear feet. Sidewalks meeting this standard may be located internal to the parking lot if separated from the surface parking, in any perimeter landscape area or landscape median, or along any through access drive.
- D. **Parking Dimensions.** Parking areas shall be designed to meet the dimension specifications in Table 7-8: Parking Dimensions.

**Table 7-8: Parking Dimensions**

Parking Angle Width (A)	Width (B)	Length (C)	Depth to Curb (D)	Curb Width (E)	Aisle Width – One-way (F)	Aisle Width – Two-way (G)	Bumper Overhang*
0°	7.0'	22'	7.0'	22'	12'	20'	n/a
30°	8.5'	20'	18'	17'	15'	20'	1.5'
45°	8.5'	20'	20'	12'	15'	20'	1.5'
60°	9.0'	19'	21'	10.5'	18'	24'	2.0'
90°	9.0'	18'	18'	9.0'	20'	24'	2.0'

\* Amount of Depth to Curb dimension that may overhang landscape area or sidewalk other wheel stop block. If overhanging sidewalk, this amount shall be added to the required minimum sidewalk width.



- A: Parking Angle
- B: Width
- C: Length
- D: Depth to Curb
- E: Curb Width
- F: Aisle Width -- One-way
- G: Aisle Width -- Two-way

**Figure 7-3 Parking Dimensions**  
Dimensions standards of Table 7-7, applied to typical parking layouts.

**E. General Design Standards.**

1. All required parking shall be on-site except as specifically provided in this Article for credits or shared parking sections. Additionally, the Director may allow for a portion of required parking off site through a site plan review subject to the following specific considerations:
  - a. It is within 300 feet of the subject site;
  - b. It is in the same or comparable zoning district;
  - c. The presence of the off-site lot does not negatively impact potential development on that lot or in the vicinity;
  - d. There are no pedestrian barriers or other access constraints;
  - e. An agreement demonstrating rights and control of the off-site property is provided.
2. No parking space shall be located where it backs into a street or through access drive except:
  - a. Residential parking in driveways, which for driveways accessing buildings and lots with 6 units or less; or

- b. On-street parking on public streets or through access drives according to the standards in 3.01.
- 3. All required parking areas shall be used solely for parking of vehicles in operating condition for patrons, occupants or employees of the use, unless specifically authorized otherwise by provisions in this code.
- 4. All parking and access areas shall be designed to adequately address drainage and runoff, including curb, gutters and inlets, or any other drainage strategy approved by the Public Works Director to support best management practices to minimize runoff and encourage infiltration of storm water.
- 5. All off-street parking areas and driveways shall be graded and paved with an all-weather material meeting Public Works Standards and Specifications.
- 6. All off-street parking spaces in parking lots shall be outlined painted stripes or other similar markings on the surface, except for parking lots with under 10 parking spaces
- 7. All non-parking spaces, such as loading zones, emergency and drive-through lanes, or spaces in front of doorways and entrances shall be clearly differentiated from parking.

**7.05 Loading Areas**

- A. **Loading Requirements.** In mixed-use, commercial, or industrial districts, off-street loading shall be required as indicated in Table 7-9, Loading Areas.
  - 1. The number and size of spaces may be revised based on the operating characteristics of the particular use and determined through site plan review.
  - 2. Loading areas shall be located on a remote portion of the building and site or internal to the block and buffered by other buildings wherever possible.
  - 3. Loading areas and activities shall not interfere with the use of walkways, drive aisles, stacking areas, internal access streets or public streets.
  - 4. Loading shall be screened from public streets or adjacent residential areas in a manner that best limits visibility and mitigates noise, according to the buffer types and design standards in Section 8.03.

<b>Gross Floor Area</b>	<b>Required Loading Area and Size</b>
<i>Under 10,000 s.f</i>	N/A, or may be shared per 7.05.B
<i>10,001 – 25,000 s.f</i>	1 space; 10' x 25'
<i>25,001 – 40,000 s.f.</i>	2 spaces; at least one of which is increased to 10' x 50'
<i>40,001 or more s.f.</i>	3 spaces, plus 1 for every 50,000 s.f. over 100,000; at least every third space shall be increased to 10' x 50'

- B. **Mixed Use Buildings or Districts.** In any area, project or zoning district designed to promote pedestrian activity, or for buildings and sites where more compact building and site design is required, alternate loading standards shall be permitted by the Director. Alternate loading standards may include sharing of loading spaces among multiple smaller tenants, using side streets, on-street parking, or alleys – particularly where there is sufficient spaces during off hours for loading or deliveries per Table 7-8, or other similar strategies that avoid designing sites for large vehicle access.

## 7.06 Alternative Compliance

- A. **Alternative Access & Parking Plans.** Alternative compliance to the access and parking standards in this Article may be authorized according to the process and criteria in Section 2.07, Alternative Compliance, as supplemented by the following specific provisions and criteria.
1. *Administrative Site Plan.* The Director may approve alternative compliance to the access and design standards or reduce the required parking or dimension standards by up to 25%, through the Administrative Site Plan process in Section 2.05.
  2. *Site Plan.* The Planning Commission may approve an alternative access and parking plans beyond what may be approved by the Director, through the Site Plan process in Section 2.05, provided it is supported by a specific traffic or parking study and industry standards.
- B. **Deferral of Required Spaces.** In any of the above cases, a portion of the required parking may be deferred through the site plan review if the initial occupancy of the premises will be adequately served by the lesser number of spaces and an approved final plan clearly indicates the location, pattern, and circulation of deferred parking. The deferred parking area shall be brought to finished grade, be landscaped, and shall not be used for building, storage, loading or other purposes. The approval of the site plan shall specify a time, criteria, or occurrences where the Director may require construction of necessary parking.